

The Cocktail Class Wooden Boat Racing Association

2014 Annual Report



CCWBRA OFFICERS

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Fred Allerton

Tom Bishop

Russ Bowler

Gretchen Granbery

Lee Urbani

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Fleet Captain Frank Stauss

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STANDING COMMITTEE CHAIRS

Rules & Procedures Rich Faulkner

Safety Fred Allerton

Design & Construction Kim Granbery

Engines Charlie Iliff

Appeals Peter Urbani

FROM THE EXECUTIVE COMMITTEE

The 2013 revision to the Bylaws of the Cocktail Class Wooden Boat Racing Association requires that the Executive Committee report annually to the Board of Directors on the status of the Association. This report provides a detailed review of our operational and financial status as of December 31, 2014. We hope that the information contained in this report will provide the Board of Directors and the membership with sufficient information to instill confidence that the Association is properly managed for the benefit of its membership, its financial structure is sound and that appropriate decisions and actions are being taken regarding the near- and long-term success of the CCWBRA.

The organization continues to grow, which presents the challenges of preserving the fun and affordable aspects of the Cocktail Class while maintaining the safety and fair play for all participating members.

See you on the Race Course!

Richard Faulkner
Commodore

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2014 YEAR IN REVIEW

The CCWBRA completed another successful year with seven highly competitive sanctioned regattas in four states including two new venues and our fourth National Championship. Racing classes were modified once again in 2014 becoming more competitive with the 6-hp Classic Class split into pre-1976 and 1976-1979 engines. A new Youth division was also added for both 6-hp Classic classes with very competent and confident young members driving competitive races. Member participation ranged from 7 boats and 10 drivers at Mid-Winter Regatta in Florida, the smallest of the regattas, to 32 boats and 54 drivers at our National Championship at Rock Hall, MD.

The Association continues to grow: membership increased from 118 to 190 people in 23 states and 4 countries. Members have registered 135 boats worldwide. The Mid-Atlantic States and the Chesapeake Bay region specifically provide the core of our membership; however, CCWBRA Fleets continue to grow, especially in Florida, with individual memberships increasing in the mid-west and mid-south regions.

An important initiative begun in 2013, establishment of the Youth Class, was completed in 2014. The CCWBRA initiated development of a youth racing program for family members aged 12 to 16. The program outline was developed by members, then approved by the Board of Directors in early 2014. Youth races were held at 2014 racing events with the intent to expand youth racing as interest continued to grow.

As interest in Cocktail Class racing has spread, several groups of members have organized local Cocktail Class fleets and have expressed interest in formal fleet recognition. The CCWBRA is working with our insurance provider to provide coverage for individual fleets to hold sanctioned events.

Our Standing Committees continue to address key issues related to racing rules, safety, appeals, design and construction, and engines.

Our *Racing Handbook*, experienced race committees, extensive pre-race planning and regatta advertising helped ensure well-run races with record numbers of boats, drivers, pit crews, volunteers and spectators. We continued to refine rules for engine “kill” switches, helmets, life jackets, and safety inspections of boats, engines and equipment, to provide a safer racing environment. Also, race courses were modified in an effort to reduce capsizes in the late model 6-hp and 8-hp classes.

The financial status of the Association is sound with a cash-on-hand balance of \$10,642.81 on December 31, 2014. Annual membership dues remained \$25 per person in 2014 and provided

the bulk of our operating funds. Additional revenue sources included proceeds from race entry fees, boat registration fees and royalty payments.

In 2014 we initiated a series of informal, unsanctioned activities following our major regattas to provide an additional day of relaxed racing and family activities. Racing rules and engines classes were relaxed to provide a forum for experimentation and to showcase member creativity. Unfortunately, a serious accident occurred resulting in driver injury. It is likely that this accident would not have occurred had our racing rules and engine classes been in effect for the day's events. Following this incident, the Safety Committee reviewed the racing rules to improve the safety of all races and race equipment. Additionally, the Executive Committee is considering a vote that no activities will be held outside the CCWBRA racing rules and engine classes. Details regarding this accident may be found in the Safety Committee Report in Appendix B.

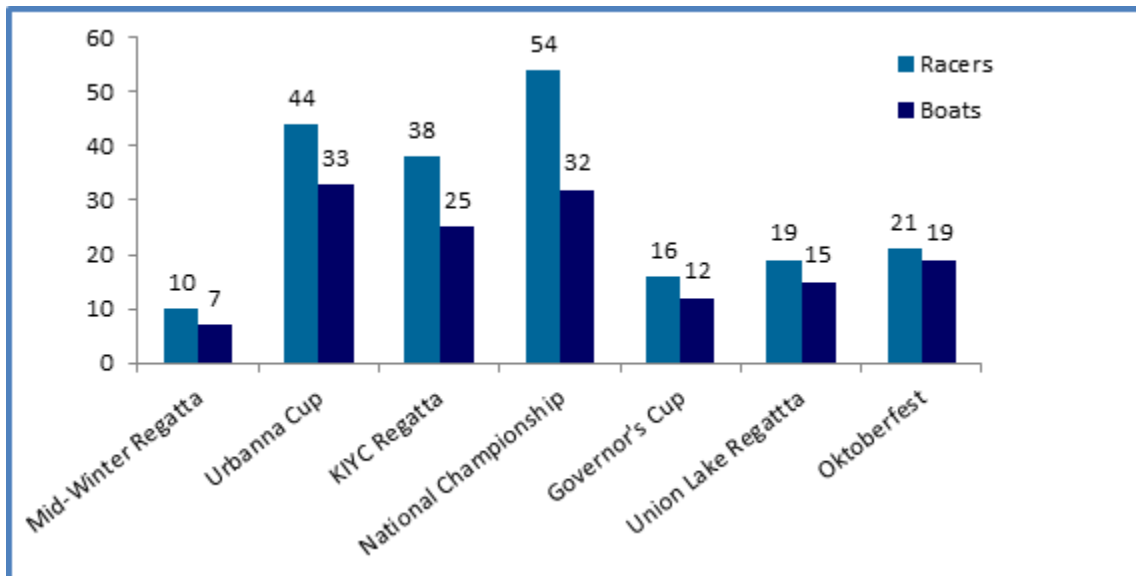
A second accident occurred during the Oktoberfest regatta which caused significant boat damage. A detailed investigation by the Safety Committee has resulted in revised rules for 2015 which we hope will significantly reduce the potential for accidents at the turn marks. Additionally, plans are being considered for several driver training workshops. Details of this accident are contained in the Safety Committee Report found in Appendix B.

With the resolution of these incidents, the incorporation of additional safety requirements and rules changes and the successful but on-going recovery of our member-driver, the 2014 season closed with no significant financial, operational or legal areas of concern.



REGATTAS AND SHOWS

The 2014 racing season was very successful as members improved the safety of regattas, added venues, defined new racing classes and witnessed increased participation. Most of the events were held in the Chesapeake Bay region, with additional races in NJ and FL. The seven regattas were held at Lake Placid, FL; Urbanna, VA; Kent Island, and Rock Hall, MD; Union Lake, NJ; and New Smyrna Beach, FL. Regatta participation continued to increase for both the number of boats and drivers with many drivers racing in multiple engine classes.



Mid-Winter Regatta – The first event of the year was an incredible getaway for those who were able to make the trip. Members from WA, IL, PA, MD, VA, GA and FL made the trip to Lake Placid, FL. CCR racing was shared at Lake Apthorpe with a few AOF hydroplane racers. The surrounding sweet-smelling orange groves in sunny, 75° weather only made it all the more wonderful.



Motor Testing Weekend – Hosted by Charlie & Lucy Iliff on the Severn River, Charlie and the Glue Factory team provided a structured approach to gathering 6-hp outboard performance data. Many member and Glue Factory engines were tested. Results and recommendations were published in the Engine Committee report available on the CCWBRA website.



Urbanna Cup – The Urbanna Fleet hosted a premier regatta on a beautiful sunny weekend. The event was well organized with shuttles to and from town, excellent food vendors, an evening band and a demanding race course. This was the first event of the year in the Chesapeake Bay area and was the 2nd largest event, surpassed only by the National Championship.



Kent Island, MD – Calm winds and water conditions made for great racing. More drivers fell out of their boats while not racing than while racing. Many first time adult and youth racers competed in the event. The Kent Island Yacht Club again hosted an outstanding event with great food and drink as well as camping on the KIYC grounds overlooking Kent Narrows.



National Championship – Our fourth National Championship held at Rock Hall Yacht Club had fantastic weather and the level of competition was worthy of a national championship event. We had members participate in the racing travel from as far away as Toronto and Georgia. The well-



organized Race and Safety Committees had their hands full with multiple heats in seven different classes for a total of 86 races in 26 heats, 6 youth races and a few tie-breaker rounds. During the Awards Ceremony a perpetual trophy was presented to the Bluefeld family to be kept at Dr. Bluefeld's house in Mollusk, VA. The plaque is the size and shape of a Cocktail Class Racer's transom with brass plates for each class of racing and winner since the first Nationals in 2011.

Governor's Cup – CCWBRA racers competed to be the first power boaters to have their names engraved on Smyrna Yacht Club's Governor's Cup trophy in 73 years. Spectators from all over the central and east coast of Florida and Intracoastal Waterway travelers (anchored in the waterway) watched the racing. A novel idling start method was used on an oval course.



The Smyrna Yacht Club plans to double the size of their fleet and host a major CCWBRA regatta in early 2015. Additionally, the Smyrna Yacht Club continues to be instrumental in heightening the interest in our organization and our brand of powerboat racing throughout Florida via their active participation in the Florida Council of Yacht Clubs.

Union Lake Regatta – Another fun and safe event that was thoroughly enjoyed by all was held at the Union Lake Sailing & Tennis Club for the fourth consecutive year. In light of the focus on safer courses for the 8-hp boats, two courses were used and well-received: an oval for the 8-hp races and a more familiar, smaller course for the 6-hp races.



Oktoberfest – Although chilly, our last regatta of the year was fun and appreciated by the racers. Eight-boat-length overlap marker buoys were used for the first time, as was the division of the Classic 6-hp engines – both were well received. Three special awards were presented to members and a CCWBRA burgee was presented to Rock Hall Yacht Club for being such avid supporters of the CCWBRA.



Shows and Festivals – Much of the success in our membership expansion has been the hard work of individual members who have organized and participated in a variety of festivals, shows, and demonstration races. Participation in the Wings, Wheels and Keels show in Topping, VA; the demonstration races at the Poquoson Seafood



Festival in Poquoson, VA; the Marine Market Demonstration in Washington, NC; and the Hospice Turkey Shoot Regatta in Irvington, VA all helped to support the continued interest and expansion of the CCWBRA. Perhaps the most significant show held in 2014 was the Mystic Seaport WoodenBoat Show held in Mystic, CT late June. More than 15 members traveled to Mystic Seaport for three days to man the CCWBRA booth, provide in-water demonstrations and host the Mystic Seaport

vs WoodenBoat Magazine Races. While a great deal of interest is generated by these events, not all are as successful as we would like. We need to take a careful look at how our manpower and financial resources are expended for shows and festivals and carefully consider in which ones we will participate.



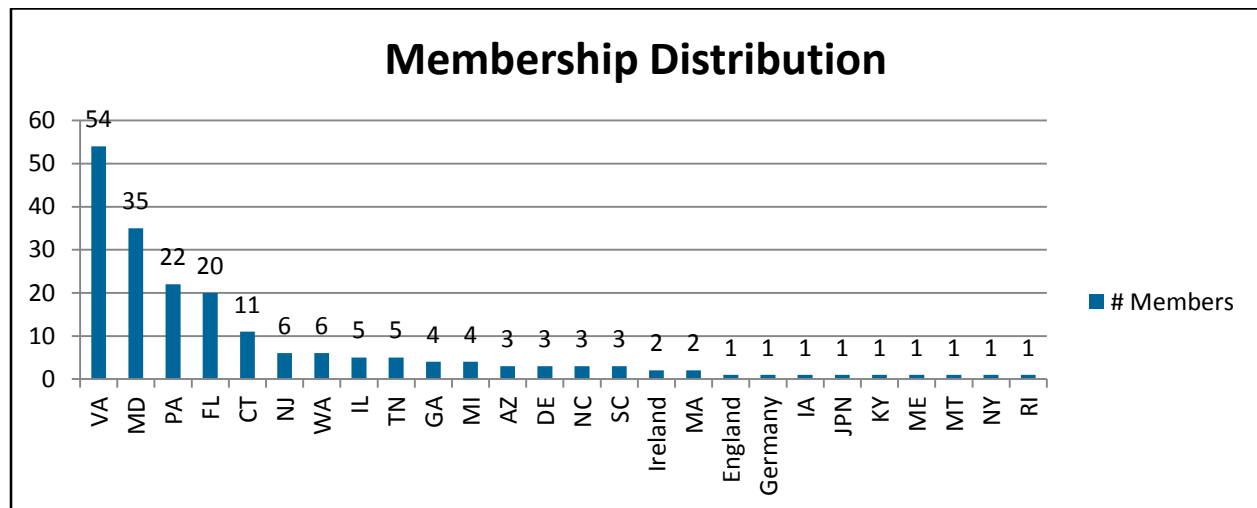
2015 Schedule – As the interest in our Association has grown, so too have requests to hold sanctioned regattas and demonstration races at clubs and festivals around the Nation. Each sanctioned regatta, demonstration race and show requires considerable effort by the Executive Committee to help organize, plan, manage costs, and run. We need to create a mechanism to identify opportunities, select and schedule events, manage race finances and conduct races that minimize the involvement of the Executive Committee and utilize local fleet volunteer assets. Our preliminary 2015 racing schedule of 7 regattas and 1 festival is shown on the inside back cover of this report.



MEMBERSHIP

Membership growth was distributed across 23 U.S. states and 4 countries. Florida was the major growth area for the CWBRA in 2014. Of the 191 members on the rolls as of December 31, 2013, only 118 renewed their memberships for 2014. Seventy-six members were added to our rolls during 2014, for a total membership count of 194 as of December 31, 2014.

As of December 31, 2014, 135 boats were registered with the Association. Ten individuals became members by purchasing CLC kits in 2014. Fifty-six of our current membership joined through the purchase of a CLC kit. Membership distribution is listed below:



Supporting our expansion nationally and internationally has been the CCWBRA website. As people find out about the CCWBRA through magazine articles, shows and races they are able to explore details of our organization and download membership information. The website provides invaluable social connectivity with our members through the member forum, photo galleries, news updates and calendar. It is the key element in our member communications and prospective member outreach program.

We will continue to try to garner attention in national and regional magazines. A 2013 article in Chesapeake Bay Magazine was instrumental in the establishment of the Smyrna Yacht Club fleet. PropTalk Magazine continues to generate interest in the Chesapeake Bay area, and regatta advertising and race coverage via local print, television and radio outlets provide excellent local and regional reporting of our activities. Additionally, a high-quality video production of the 2014 Urbanna Cup is frequently used by members to generate interest in our organization and our style of racing. The video is available on our website.



OUR ORGANIZATION

The CCWBRA's formal organization was established in our Association's Bylaws, as revised in December 2013. Our Executive Committee and Board of Directors oversee the operation of the CCWBRA. We have five standing committees to support the areas deemed crucial to the continued safety, fun, fairness and performance of our members and their boats: Racing Rules & Procedures, Safety, Design & Construction, Engines, and Appeals. Reports from these Committees are provided in Appendix B.

From an organizational perspective, we need to address the Secretary Position responsibilities. BreeAnn has done a fantastic job keeping the organization informed and organized. The workload for the Secretary has steadily gotten heavier due in part to the following factors; membership continues to grow, the increasing number of sanctioned events, and the higher number of participants at many of the sanctioned events. The Executive committee needs to evaluate options such as splitting the position responsibilities in order to maintain the position as a part-time volunteer position.

In the upcoming year (2015) the organization will need to elect new members for several Board of Directors and Executive Committee positions. Per the CCWBRA Bylaws, Directors serve a three-year term and Executive Committee members serve a two-year term. A nominating committee will be appointed to provide a list of nominees to the membership for voting via email.

		Board of Directors				
		Fred Allerton	Tom Bishop	Russ Bowler	Gretchen Granbery	Lee Urbani
Term	Begin	2014	2014	2014	2012	2012
	End	2017	2017	2017	2015	2015

		Executive Committee						
		Rich Faulkner	Todd Steffes	Fred Bluefeld	BreeAnn Edmonds	Frank Stauss	Mike McNerney	Curt Bluefeld
		Commodore	Vice Commodore	Treasurer	Secretary	Fleet Captain	Member-at-Large	Past Commodore
Term	Begin	2014	2014	2014	2014	2014	2014	2014
	End	2015	2015	2015	2015	2015	2015	2015

Our State Fleet Captains serve as local points-of-contact for members within the state, assist with coordinating local races and shows, and support the growth of the Association. State Fleet Captains have been instrumental in identifying opportunities for demonstration races, displaying our boats and generating interest in our organization.

Local racing fleets have developed in several locations across the country. Currently, there is no mechanism for formally recognizing and supporting local fleets. Discussions with our insurance

company are fairly close to finalization to provide accident and injury insurance coverage for sanctioned CCWBRA events, as well as basic coverage for local officers. The cost for local insurance premiums to cover the local officers is in the works as well.

Urbanna, VA – The Urbanna Creek Cocktail Class Boat Association was formed in 2013 by a small but dedicated group of boat builders and racers. In just two years, they have firmly established the Urbanna Cup as one of the premier events for the CCWBRA. Their annual race was the 2nd largest for the Chesapeake Bay region and we anticipate high participation at this event for years to come.



New Smyrna Beach, FL – The Smyrna Yacht Club (SYC) in New Smyrna Beach, FL had only a handful of members building boats in 2013. Within a year they had held their first regatta, re-establishing the 73-year-old Governor Dave Sholtz Trophy for Powerboat Racing as one of the premier racing events in the CCWBRA. More than a dozen entrants from Maryland to Florida competed in their inaugural 2014 regatta on the Florida Intra-Coastal Waterway including the first woman to race in the 8-hp class. SYC members are in a building frenzy and plan to double the size of their fleet in 2015 and hold the second Governor’s Cup regatta for CCWBRA as a mid-winter race in February 2015. Expectations are high with more than 20 boats and 30 drivers expected from as far north as CT and as far west as TN. The SYC has also been active in promoting CCWBRA racing throughout Florida via their active participation in Florida Council of Yacht Clubs. Their efforts have been very successful with interest expressed from multiple yacht clubs on both the east and west coasts of Florida.



CONTRACTS AND MEMORANDUMS OF AGREEMENT

The CCWBRA has established two memorandums of agreement (MOA) for the right to use our trademarked names **Cocktail Class Wooden Boat Racing Association** and **Cocktail Class Racer** and any inference to them. The first MOA was established in 2012 authorizing Chesapeake Light Craft (CLC) to produce and sell a Cocktail Class Racer kit and plans on a commercial basis. A royalty of 3.8% is paid for each full kit sold with a reduced royalty for partial kits and plans. With each kit sold, the CCWBRA provides a one-year membership. This has resulted in over 56 new members worldwide. Our relationship with CLC has been close and productive and they have been a consistent and strong supporter of our Association.

An MOA with the Bridgeport, CT Public School System was established in 2013 authorizing the school system to build and race Cocktail Class Racers on a not-for-profit basis as part of their Aquaculture Science and Technology Education Center's curriculum. As a not-for-profit organization, assessment of royalties was not included in the MOA. Six boats were built during the 2012-2013 school year with a formal regatta held in June 2013. Boats were built and raced by students 14 to 16 years in age in 2014 with plans to continue the program in 2015.

During the last two years several non-profit organizations and for-profit business have used our trademarked name to help sell products and services or to increase their memberships or activities. As these individuals, organizations and businesses are identified we will seek to establish memorandums of agreement and royalty payments where appropriate. When necessary, we will aggressively protect our Association's trademarked names and logo and our now-international powerboat racing brand.

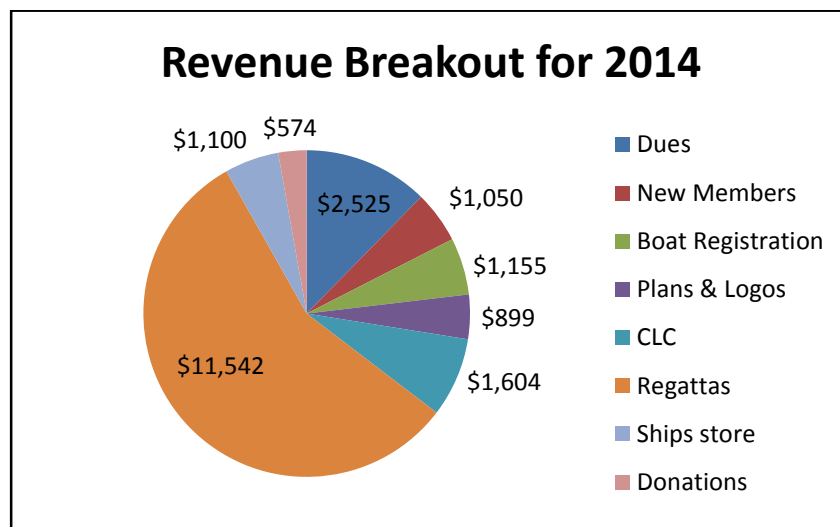


FINANCIAL STATUS

The CCWBRA began the year with a balance on hand of \$6,967.55 and ended the year with a balance of \$10,642.81. Growth of the organization through new members, registrations of new boats and royalty income from the interest in CLC kits all contributed to the financial success of the Association in 2014. Our annual insurance premium of \$3,295 was paid in April and although it represents our largest and most important expenditure for the Association it was the same in 2014 as in 2013. To maintain our web hosting service and retain rights to our domain name www.ccwbra.com, \$254.78 was expended. Appendix A provides a detailed accounting of the CCWBRA's income and expenses as of December 31, 2014.

The organization's expenses remained relatively consistent in 2014 with 2013 expenses while income increased by 15% above 2013 revenues. Surplus funds more than doubled.

	<u>2013</u>	<u>2014</u>	<u>Difference</u>
Income	\$17,732	\$20,448	\$2,716
Expenses	\$16,058	\$16,743	\$685
Profit	\$1,674	\$3,705	\$2,031



Expenditures for 2015 are expected to increase over 2014. Our third annual liability insurance premium will come due on April 1st and is expected to be 20% higher due to the growth of the organization and the addition of races during the winter and proposed new races during the year. In addition to the ongoing web hosting expenditures, the Association is exploring the possible

purchase and use of a standardized accounting format that will lend itself to producing an annual audit.

Expectations are that the CCWBRA will grow beyond the original financial levels of activity and balances established in our agreements with Wells Fargo and PayPal. These new levels of activity and balances will most likely result in and contribute to an increase in overhead expenses in 2015. Costs associated with our regattas are covered by race entry fees, which over the years have covered all costs with a small portion of the race fee provided to the general ledger to cover insurance and overhead. The interest in and growth of the CCWBRA may present additional new organizational expenditures to be included in the proposed budget for 2015. The Board of Directors will consider these changes following inputs from a review of this Annual Report.

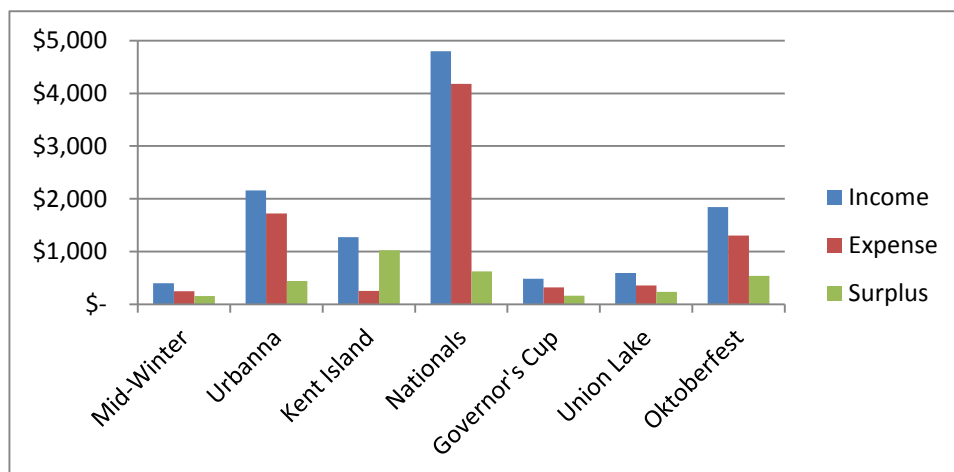


OUR COURSE FOR 2015

The fifth year of the CCWBRA promises to be one of continued growth both in terms of membership as well as racing. We plan to offer new racing venues to our members in the Chesapeake Bay region, expand our youth racing program, formally expand our network of racing fleets, and offer members outside the Chesapeake Bay increased opportunities to fully participate in sanctioned racing. We should be able to run additional sanctioned races in other areas by working with interested local clubs and members to organize, advertise and run at least two events outside of the Mid-Atlantic region. Use of surplus revenue may be considered for sending experienced race committee volunteers to support new sanctioned events on the west coast. As this is a non-operational use of funds, it is anticipated that the Board of Directors will need to approve this use of funds.

Given the serious incident we experienced this year, the CCWBRA will continue working with the Puget Sound group using the Mercury motors to develop safe racing procedures for these classic engines. Factors to be considered include but are not limited to start, course layout and venue size to accommodate the faster Mercury motors. As the Mercury motors are more readily available in the Puget Sound area it is incumbent on the CCWBRA to assist members in adapting existing rules and racing procedures to safely conduct Cocktail boat racing there.

As a not-for-profit organization, the Executive Committee will need to review how to utilize the surplus funds to further the goals of the CCWBRA or reduce the race fees to maintain the family oriented/affordable racing we originally set out to provide members. The organization has experienced extraordinary growth in the past few years. We need to assess whether the current revenue model is appropriate for our organization to weather years of flat or declining membership. The following chart depicts the funds generated by each regatta which was the largest contributor to the surplus. Regattas are also the largest expense item for the organization which resulted in total surplus funds from Regattas of \$3,171. The overall CCWBRA budget year-end surplus was \$3,705 which indicates the Regatta funding as the major source of surplus funds.



APPENDICES

A – 2014 Financial Report

B – Standing Committee Reports

- **Racing Rules**
- **Safety**
- **Design and Construction**
- **Engines**
- **Appeals**

C – Youth Racing Program

D – Inventory of Equipment and Assets

Appendix A - 2014 Financial Report

Income

Membership Dues		2,525.00
New Member Registrations		1,050.00
Boat Registrations		1,155.00
Building Plans & Logo Decals		898.50
CLC Royalty		1,604.47
Regattas		
Mid-Winter Florida Regatta	400.00	
Urbanna Cup	2,160.00	
KIYC Regatta	1,270.00	
National Championship	4,800.00	
Governor's Cup	480.00	
Union Lake Regatta	590.00	
Oktoberfest	1,842.00	11,542.00
Ships Store		1,100.00
Donations		574.00
Total Income		20,448.97

Expenses

Administration		
Postage	426.05	
Printing	1,188.15	
Logos	1,496.20	
General Operations	714.54	
Regatta Equipment	105.99	3,930.93
Insurance		3,295.00
Regattas		
Mid-Winter Florida Regatta	245.00	
Urbanna Cup	1,720.00	
KIYC Regatta	249.55	
National Championship	4,178.00	
Governor's Cup	320.00	
Union Lake Regatta	353.79	
Oktoberfest	1,304.44	8,370.78
Ships Store		1,147.08
Total Expenses		16,743.79
Net Income/Loss		3,705.18

Appendix B – Standing Committees’ Reports

Racing Rules Committee Report

December, 2014

The Racing Rules Committee for 2014 included members Rich Faulkner, Frank Stauss, Gretchen Granbery, Lee Edmonds, Todd Steffes and Jim Schmicker.

The committee has reviewed proposed change to the Racing Rules coming from various sources including committee members, other committees and interested members. Business has been conducted through group emails.

The Racing Rules were stable through the season without any significant changes. Adaptations to race course marking including overlap zone reference marks and stand-off marks at the start-finish line have been experimented with. The maturation of the Racing Rules is evident by the limited number of proposals under consideration for 2015.

The proposals coming out of the Racing Rules Committee for Executive Committee action are:

1. Sportsmanship. Add a section to the Handbook stating the need for adherence to practices of good sportsmanship and add provisions to allow for action by Race Directors, after hearings by Protest Committees, to penalize transgressors of the sportsmanship standards. Further review by the Executive Committee in instances of penalties is recommended.
2. Collisions - Placing responsibility on right-of-way boats to avoid collisions or face penalties when damage occurs.
3. Holding Area – The delineation of a holding area adjacent to the start line (if race venues permit) and the requirement for drivers to be in it immediately prior to the first race of their heat.
4. Premature Starters – After feedback from drivers and much discussion the committee proposed some changes to the scoring and how to handle premature starters:
 - a. Any boat with any part of its equipment over the line prior to the starting signal that also finishes the race shall be awarded points equal to DNF plus 3 points.
 - b. Boats that are over early that safely retire from the race receive a DNF plus 1 point.
 - c. Boats starting correctly will receive a score of their finish position less one point for each premature starter that finishes the race in front of them.

Respectfully submitted,
Jim Schmicker
Racing Rules Committee Chair
February 8, 2015

Safety Committee Report

December, 2014

The Safety Committee includes members Lee Edmonds, Tom Kerr, Ken Tweed, and Fred Allerton. The business of the Safety Committee was conducted by numerous call-in meetings and group emails.

Issues and activities in which the Committee was involved included the following:

1. Safety inspections were conducted for boats registering for their first event of the season. The Safety inspection checklist was updated for the 2014 racing season to monitor compliance with current safety regulations.
2. Incidents are summarized below. Beginning in 2013, Incident Reports were filed to monitor safety issues and to help recommend informed policy decisions.
 - a. The number of boat capsizes was down slightly from 2013. No injuries resulted from any capsizes.
 - b. By far, the most serious safety incident of 2014 was the accident involving a member using a 1952 7.5 horsepower Mercury KG-4H on August 17.

CCWBRA is extremely grateful to the many individuals (and organizations) that were critical links to the driver's rescue, medical care, and remarkable recovery progress: three good Samaritans who rescued the driver from the water, CCWBRA safety boat crew, including Pete Urbani (EMT), Rock Hall FD Rescue Squad, MD State Police Aviation Trooper 1 (MedEvac), University of MD R. Adams Cowley Shock and Trauma Unit, and especially the driver's wife and family.

Due to the serious nature of the driver's injuries, MD Department of Natural Resources Police investigated the accident, and filed a report which is accessible to the public. Although very comprehensive, the MDNR investigation drew no conclusions regarding contributing factors or cause.

The accident occurred during the "Bootlegger" race, which was a non-sanctioned event held the day after the National Championship Race at Rock Hall Y.C. The accident served to focus attention on a number of points regarding the conducting of non-sanctioned events, which are under consideration for policy decisions:

- "Non-sanctioned" events should be conducted under the same guidelines as "sanctioned" events, as far as items such as: safety and equipment rules and procedures, safety plan, and emergency readiness.

- Engine horsepower limitations for “non-sanctioned” events should be comparable to limitations for “sanctioned” events.
 - The severity of this accident and nature of the injuries was a strong reminder of the importance of a helmet standard that’s intended to reduce the risk of head injury (full-cut profile and Snell or DOT standard).
- c. Boat contact continued to be a concern during the 2014 Season. Examples:
- Two collisions at two different events resulted in structural damage to one boat involved in each incident.
 - Protests were filed in two separate events by overtaken boats that were involved in collisions near turns. Protest Hearings considered the overtaking boats to be overlapped in each case, and found the overtaken boats to be at fault.
 - Competitors are expected to promptly report infractions, including minor boat contact; although such infractions may sometimes go unreported. At the Union Lake race, boats running in first and second position in one of the final heats made minor contact with each other – twice. First by one driver and then by the other. In a show of good sportsmanship, the two drivers reported the infractions. Both took a penalty and, as a result, changed their standing in the final results of the race.
3. Seven *proposals* for Safety Rule revisions were submitted to the Executive Committee for consideration as part of the 2015 edition of the Rules Handbook. Included among the proposals were: revisions to rules for helmets, and bow and transom handles; a rule regarding steering/throttle failures during a race; a rule changing the forward location for the “kill” switch from a *recommendation* to a *requirement*; and an expansion of the driver eligibility rule.
4. Several *suggestions* for Racing Rule revisions were submitted to the Racing Rules and Procedures Committee for consideration. Included among the suggestions were:
- a. A recommendation to adjust the minimum distance from a turn mark for establishment of a legal overlap from 4 boat lengths to 8, acknowledging the difficulty for drivers to judge the shorter overlap rule radius. Members of the Safety Committee also recommended marking the 8 boat length zone with an OLR (overlap rule) buoy. A test of this modified arrangement was conducted successfully at the Oktoberfest race. And subsequently, recommendations were submitted for revisions to the applicable racing rules.
 - b. A recommendation for revisiting “Rules of the Road” to include an “Avoiding Contact” rule and minor revisions to the “Overlap” rule. Members submitted these recommendations in part to help put the focus on safe overlaps and contact avoidance, and to check unsafe driving tactics in overlap situations near turn marks.

5. During the 2015 racing season, the Committee looks forward to collaborating with other committees on initiatives to improve racing safety, such as promotion of safe-driving with education materials, training sessions, and enforcement.

Respectfully submitted,
Fred Allerton
Safety Committee Chair
February 5, 2015

Design & Construction Committee Report

December, 2014

The Design and Construction Committee includes members Keith Carew, Lee Edmonds, Chris Riddick, Morgan Friday and Kim Granbery. Business has been conducted primarily through group emails and in person. Issues and activities in which the Committee was involved included the following and will be added to the 2015 *Racing Handbook* and the *Building Manual*:

1. Limiting the use of Exotic Materials.
2. Establishing a minimum fuel tank size.
3. Adopted a keel modification to reduce ventilation.

There are currently no outstanding issues that the Committee foresees addressing this year. At this stage of our growth it appears as though the one-design mission of our Association, from a design and construction basis, is strong and stable.

Respectfully submitted,
Kim Granbery
Design and Construction Committee Chair
February 4, 2015

Engines Committee

December, 2014

After some testing, observation at competitions and discussion, the Engines Committee prepared and submitted to the Executive Committee recommendations for:

1. Definition of characteristics required for legal competition in the 8-hp class,
2. Definition of the word “stock” where required in the rules for engines,
3. Definition of “stock” where required in the rules for propellers, and
4. Recommendation of further split in the classic-motor classes into those before 1976 and those manufactured in 1976-1979.

For 2015, there are no particular objectives yet established other than to monitor the competition under the new rules and respond to inquiries or issues as identified by the Executive Committee.

Committee members for 2014 and continuing into 2015 are Kim Granbery, Russ Bowler, Lawrence Fuccella and Charlie Iliff. Anyone wishing to be appointed to the Engine Committee is invited to advise the Grand Poobah.

Respectfully submitted,
Charlie Iliff
Engines Committee Chair
February 4, 2015

Appeals Committee Report

December, 2014

The 2014 racing season saw our first series of protests ranging from minor infractions involving right-of-way at the turns to serious rules infractions involving significant damage to boats. In all cases, the Protest Committee completed thorough and fair investigations with results fully accepted by all concerned.

Two issues were raised regarding the protest process, which are being addressed in the Appeals Committee:

- The protest process is time-consuming and results in racing delays. During major regattas time is a critical commodity due to the large number of racers and engine classes. A mechanism for completing the process without impacting the remaining races needs to be addressed.
- All protests heard during the season were conducted by highly experienced people. Not all regattas will have such individuals on the Protest Committee.

The Appeals Committee will develop a detailed protest procedure and train members in the process so that any regatta will have the capability to conduct quick and fair protest hearings. Committee members for 2014 and continuing into 2015 are Peter Urbani, Tara Carew, and Jim Schmicker.

Respectfully submitted,
Peter Urbani
Appeals Committee Chair
March 10, 2015

Appendix C - Youth Racing Program

After its first year, the CCWBRA Youth is off to a fine start. The only disappointment was there weren't more youth participants. Otherwise the program worked almost perfectly. Everyone was amazed how well all the kids did. There were several reasons for this – our youth took their opportunity to race seriously, they listened to their “coaches,” and they showed they were just as capable as most of the adult racers.

However, the Youth Program Committee feels there could be a couple modifications to the program that would make it even better. As a result of the success mentioned above, the division into a Youth I and a Youth II class is unnecessary. Even the youngest participant (12 years) performed quite well. Likewise, when all the participants in a particular race or heat have some racing experience, limiting the races to 2 drivers at a time may be unnecessary. The Race Director should have the discretion to increase race size to 3 boats when conditions warrant. Finally, in the interest of time, we should encourage all youth (families) to use the same “classic” engine, so as to keep from having to races in both the early and late classic classes. The Committee recommendation is to canvas current youth families to see if they prefer early or late classic engines and then make that the required engine. This may require some to have to borrow an engine to race for a while, but in time hopefully families entering the program will come in knowing what engine to have.

Finally, the Committee feels parents of potential Youth Class drivers should be encouraged get their young drivers familiar with their CCR's before attending their first race, so coaches can concentrate on teaching safe racing techniques versus the basic functions of racing a CCR.

Respectfully submitted,
Jack Pettigrew
Youth Program Chair
February 11, 2015

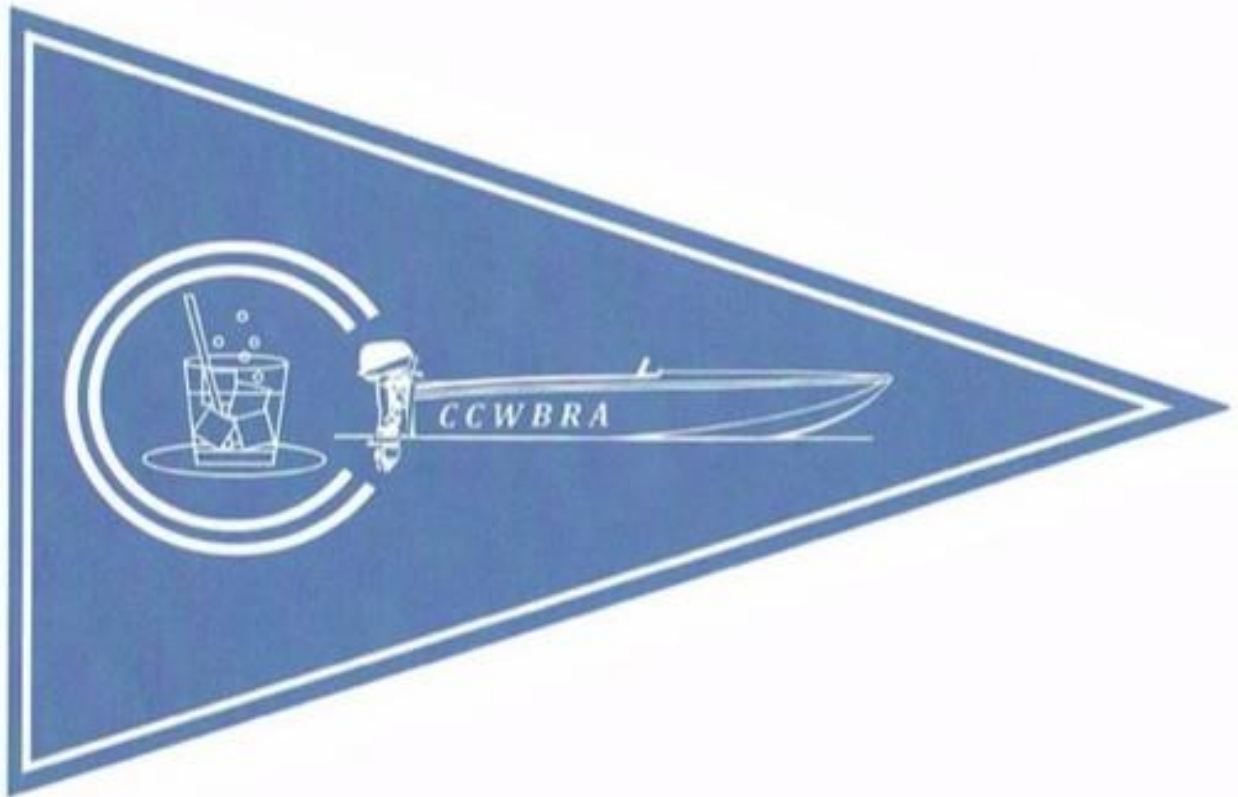
Appendix D – Inventory of Equipment & Assets

Item Description	Custodian
Automatic race starting horn	Todd Steffes
VHF handheld radios and charger	Todd Steffes
Race buoys and anchors (4)	Todd Steffes
Starting Flag set with poles	Todd Steffes
Starting flag set w/o poles	Todd Steffes
Fire Extinguisher	Todd Steffes
First aid kit	Todd Steffes
Air horns	Todd Steffes
Clipboards	Todd Steffes

PRELIMINARY 2015 REGATTA AND SHOW SCHEDULE

<u>Event Name</u>	<u>Location</u>	<u>Date</u>
Governor's Cup	Smyrna Yacht Club New Smyrna Beach, FL	February 28
Mid-Winter Regatta Lake Placid	Lake Apthorpe Lake Placid, FL	March 3
3 rd Annual Urbanna Cup	Urbanna, VA	May 16
Kent Island Regatta	Kent Island Yacht Club, Kent Narrows, MD	July 18
National Championship	Rock Hall Yacht Club, Rock Hall, MD	August 15-16
Union Lake Regatta	Union Lake Sailing & Tennis Club Millville, NJ	September 19
Mid-Atlantic Small Craft Festival	St. Michaels, MD	October 3
Oktoberfest	Kent Island Yacht Club, Kent Narrows, MD	October 17





CCWBRA's Burgee